



SOLING ONE METER CLASS

AN AMYA RECOGNIZED ONE DESIGN MODEL YACHT

Welcome to the Soling 1 Meter class, the largest class in the American Model Yachting Association (AMYA).

Your Registration Card and Hull Marker are attached. The Hull Marker with your Sail Number must be affixed inside the boat where it can be seen through the hatch opening. Please seal the sticker with a layer of clear packing tape to keep water out. You will want to keep your Registration Card handy because it must be presented at AMYA sanctioned events. It must also be returned to transfer the registration if you sell the boat.

A copy of the Class Rules is enclosed. The most up-to-date version can always be found on the AMYA web site at www.modelyacht.org. The Class Rules include Control Drawings detailing the location of various components. More information about building and sailing the S1 M can be found on the class website at www.solingonemeter.org, and on my personal website at www.willings.net.

Your Sail Number is shown on the Registration Card and must be displayed on the sail as shown in the Victor kit plans or Sail Control Drawing. The numbers must be a minimum of 3 inches high with a stroke width of 3/8 of an inch. You are only required to display the last two digits of the sail number, which enables you to fit it on the sail more easily. If you use only 2 digits you are required to leave space for the numeral '1' to be placed in front in case duplicate numbers are entered in an event.

If you have any further questions please contact me directly. (As I am serving the class in a voluntary capacity, I would always prefer that you communicate with my by email, unless there is a need for my *urgent* attention).

Wishing you navigable water, warm breezes, full sails and nimble thumbs,

Tim Willings, Class Secretary / Registrar
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Soling One Meter

Description

The Soling One Meter is a low-cost, kit-based one-design class targeted toward getting new skippers and hobbyists involved in model yachting. The Soling One Meter Class philosophy is to provide a low-cost one-design model suitable for introducing newcomers to the hobby, while offering enough performance to keep long-time model sailors interested. All aspects related to performance are intended to be restricted to what can be achieved by building the kit straight from the manufacturer's plans. Other aspects not affecting performance are not as strictly controlled.

The Soling One Meter Class was introduced to the AMYA in 1993. Since that time it has become the largest class in the organization, with just over 800 boats registered in late 2006. The Soling One Meter also qualifies in the open design US One Meter Class.



Solings must be built from a kit available from a single manufacturer, Victor Model Products of Downey, CA. The manufacturer and others offer partially or fully assembled yachts. The hull and deck are vacuum formed styrene plastic, the spars are wood, sails are flat and unpaneled. Despite these limitations, the boat sails well, satisfying even veteran skippers with its balance and handling.

The Soling One Meter class is managed by the AMYA Soling One Meter Class Secretary and an appointed Class Advisory Committee. The Soling One Meter Class Rules are posted on the AMYA web site at <http://www.modelyacht.org/s1m.html>. The Soling One Meter Class maintains a web site at: <http://www.solingonemeter.org>.

Specifications:

LOA	39.38 inches
Displacement	10 pounds minimum
Sail Area:	592 sq. inches
Draft:	Approx. 10 inches
Mast height:	52 inches
Overall height:	65 inches

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AMYA Soling One Meter Class Rules

Effective October 1, 2006

Underlined text added October 1, 2006 M1 & M2. {sample}

Struck-through text deleted October 1, 2006 M1. {sample}

1.0 Concept

The definitions, dimensions, limits, and restrictions listed are intended to maintain the one-design concept of this class. The concept behind the Soling One-Meter class is that a first-time skipper should be able to build and sail a model yacht that has essentially no performance differences from a boat built by an expert. All aspects related to performance are intended to be restricted to what can be achieved by building the kit straight from the manufacturer's plans. Aspects not affecting performance are not as strictly controlled. These rules are intended to insure that all boats are as close as possible with regard to hull, deck, keel, rudder, sails, displacement and ballast. Any obvious attempt to negate or violate this concept shall require the boat be barred from competition until such time as the violation is corrected. {*Changed 2006 M1}

1.1 General

The class specification is defined by these rules, the manufacturer's plans, and any applicable rules of the AMYA, in that order. If a feature which may enhance performance is not shown on the manufacturer's plans and not specifically allowed by these rules, then it is prohibited. All dimensions shown on the manufacturer's plans are to be adhered to unless specifically overridden by these rules. {*Changed 2006 M1}

2.0 Standard

The class shall be called the "Soling One Meter". Boats conforming to these rules must be built from a kit, herein referred to as "The Kit", obtained from an approved manufacturer. The Class Secretary shall maintain a list of approved manufacturers of The Kit.

2.1 Hull and Keel

The Hull and Keel shall be as supplied in the Kit. The Keel can be removable or permanently attached to the Hull in the location shown on the control drawing.

2.2 Deck, Hatch, and Lazarette

The deck shall be that supplied in The Kit. Any method of deck attachment is permitted, provided that the deck inboard of the hull is unchanged. The deck flange, if any, may be removed. An alternate hatch cover may be fabricated, but must conform with that supplied by The Kit. One opening (hole) may be in the deck area over the rudder shaft horn assembly. This opening, to be known as the Lazarette, is optional. If it is utilized, the opening in the deck is restricted in size to a maximum of nine (9) square inches. The actual shape of the Lazarette and its covering material is uncontrolled. {*Changed 2003 M7}

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2.3 Rudder

The rudder shall conform to that supplied in The Kit. The mounting location shall be as shown on the control drawing. In the event of damage to the original rudder, a replacement rudder may be fabricated as long as the replacement rudder shall be of the same size or smaller than that supplied in The Kit and conform to the original shape.

2.4 Interior Construction

The construction, layout, materials, and equipment used inside the hull is unrestricted as long as it does not conflict with any other rule.

3.0 Displacement and Ballast

The MINIMUM ready-to-sail weight of the yacht shall be 10 pounds. The ready-to-sail weight shall include the radio receiver, batteries, steering servo, sail control unit, sails and rigging.

3.1 Materials

Ballast shall consist of lead shot permanently bonded in the keel. Molded, solid lead ballast shall be prohibited.

4.0 Spars

The mast and booms shall be made of solid wood or plywood. Hollow spars are prohibited, although a slotted mast is permitted. If used, plywood must have all layers of uniform density. All replacement spars shall not exceed the dimensions of the originals contained in The Kit, except that the jib boom may be up to 15-1/2 inches long. No weight shall be added to the jib club forward of the swivel. {*Changed 2006 M1}

5.0 Rigging

The use of commercially available or home made ~~fittings~~, fairleads, turnbuckles, screw eyes, eye bolts, tangs, bowsies, goosenecks, boom vang, mast jacks, mast cranes, outhauls, etc. and woven or braided wire for shrouds, shall be permitted. Fairleads (sheet exit guides) shall not extend higher than one-half inch from the deck. Larger or through-bolted screw eyes may replace kit supplied screw eyes. {*Changed 2000 & 2006 M1}

5.1 Standing Rigging

The use of multiple diamond rigging shall be permitted. A permanent back stay is required. The ends of the spreaders shall not extend beyond the width of the hull at the mast. Spreader shall be made of wood, aluminum or brass. Spreader shall not be angled fore or aft of the mast. The shrouds descending from the outer ends of the spreaders shall attach either to the mast as shown in the kit plans (the diamond stays) or to the deck in the range of the General Configuration Control Drawing Note 4. If diamond stays are used and located as shown in the kit plans, a second set of stays may be attached between the spreaders and the deck in the range of the General Configuration Control Drawing Note 4. {*Changed 2001 M1 & 2006 M1 & M2}

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5.2 Mast Crane - Backstay Bracket

The crane at the top of the mast may be longer than the one supplied in The Kit and/or mounted at an angle as shown on the control drawing to prevent the mainsail from interfering with the backstay. The mast crane may be constructed of wood, aluminum, or brass. The lower end of the backstay may be attached at, but not beyond the transom.

5.3 Mainsail Height

The maximum height of the mainsail from the deck shall not exceed 51-¼ inches.

5.4 Jib Stay Attachment

The height from the deck to the jib stay attachment on the mast shall not exceed 45-¾ inches, including the jack screw, if used.

5.5 Wind Indicators

The use of a wind indicator or vane on the top of the mast shall be permitted, and shall not be included in the restriction of 5.3.

5.6 Deck Layout

All positions of deck hardware must conform to the [Soling One Meter General Configuration Control Drawing revision dated February 14, 2005](#). The method of attachment to the deck of any hardware is not controlled. [Racks may be used on the deck in place of screw eyes](#). If fittings exist for alternate positions not permitted by the control drawing, the legal positions shall be clearly marked.

{*Changed 2006 M1}

5.7 Mast Step

The mast must be stepped on-deck, but any mast step arrangement is permitted.

5.8 Running Rigging

[Any outhauls, cunninghams and halyards shall each be attached to a single spar. The use of a separate jib halyard is permitted. Topping lifts are not permitted.](#)

{*Added 2006 M1}

6.0 Sails

Sails shall be single-panel made from woven cloth of polyester fiber only, with a weight of approximately 3 ounces per sailmaker's yard. Sails must be cut to match the [Soling One Meter Sail Control Drawing revision dated February 14, 2005](#). {*Changed 2001 M2 & 2006 M1}

6.1 {*Deleted 2000}

6.2 Battens

Mainsail: No more than 3 battens positioned in such a way that the leach is divided into 4 equal parts. Maximum batten lengths: top 5 inches, middle 6 inches, bottom 4 inches.

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Jib sail: No more than 2 battens positioned in such a way that the leach is divided into 3 equal parts. Maximum batten lengths: top 4 inches, bottom 2-1/2 inches.

6.3 Sail Reinforcement

The sails may be reinforced by addition of woven cloth or tape material within 3 inches of the head, tack, and clew corners, and within 1/4 inch of the leech edge.

6.4 Sail Numbers and Class Logo

Sail numbers shall be a minimum of 3 inches in height and 3/8 inches in stroke width. They shall be placed as shown on the control drawing. The class logo shall be optional, but if present, must be as shown on the control drawing.

Alternatively, sails may be marked according to the current Racing Rules of Sailing. (*Changed 2001 M3)

7.0 Radio

A maximum of two channels shall be ~~allowed~~ used. One channel shall be used for sail sheet control only, and one channel shall be used for rudder control only. The use of a backstay tensioner, and extra jib trimmer, or jib twitcher, ~~etc.~~ is prohibited. Any brand and type of radio equipment is permitted. Transmitters and receivers may have more than two channels, provided only two channels are used. {*Changed 2006 M1}

8.0 Adhesives

The use of any adhesive is permitted to bond any part provided with the kit, or allowed by these rules. {*Added 2006 M1}

Revision: October 1, 2006

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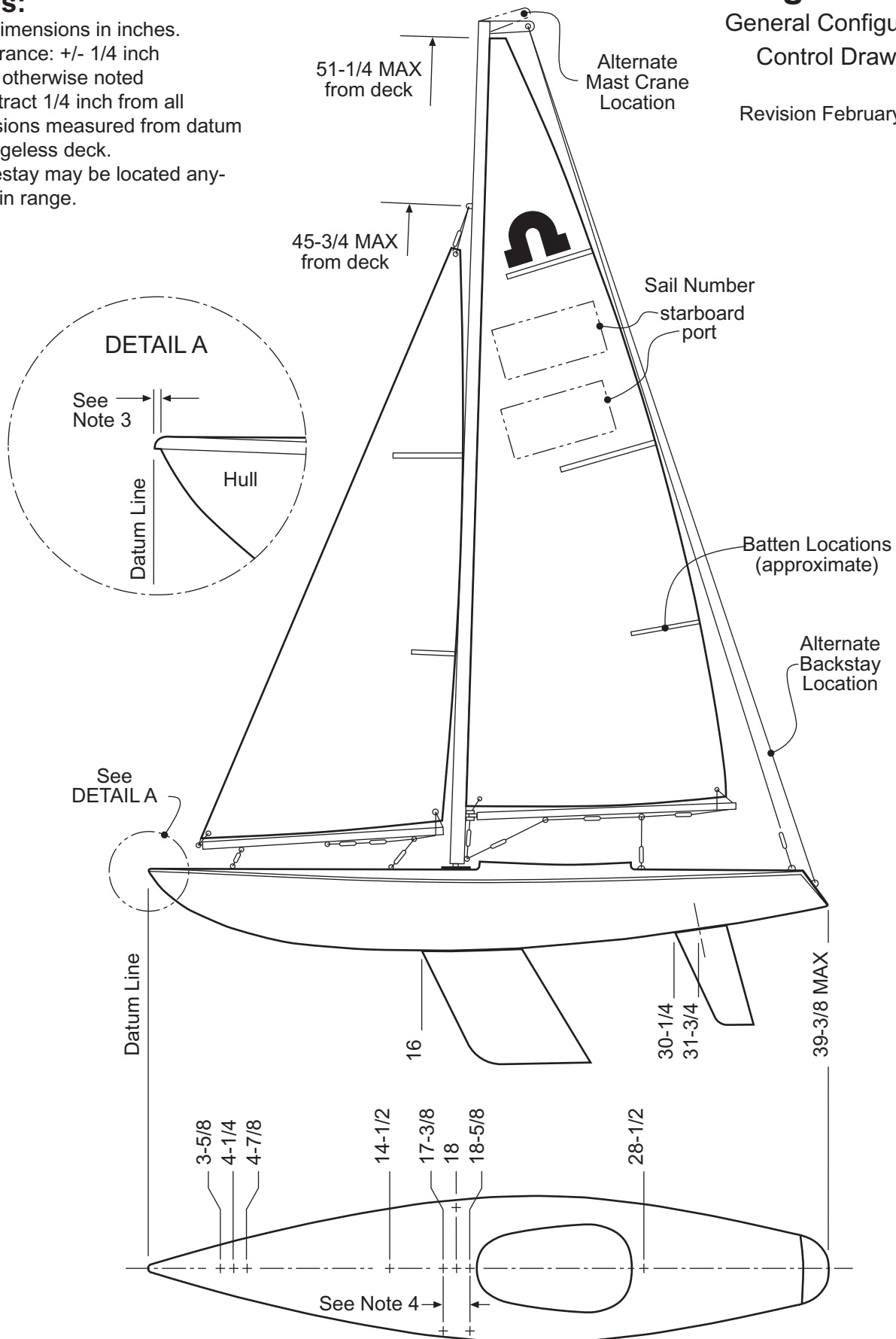
General Configuration

Control Drawing

Revision February 14, 2005

Notes:

- 1) All dimensions in inches.
- 2) Tolerance: $\pm 1/4$ inch unless otherwise noted
- 3) Subtract $1/4$ inch from all dimensions measured from datum for flangeless deck.
- 4) Sidestay may be be located anywhere in range.

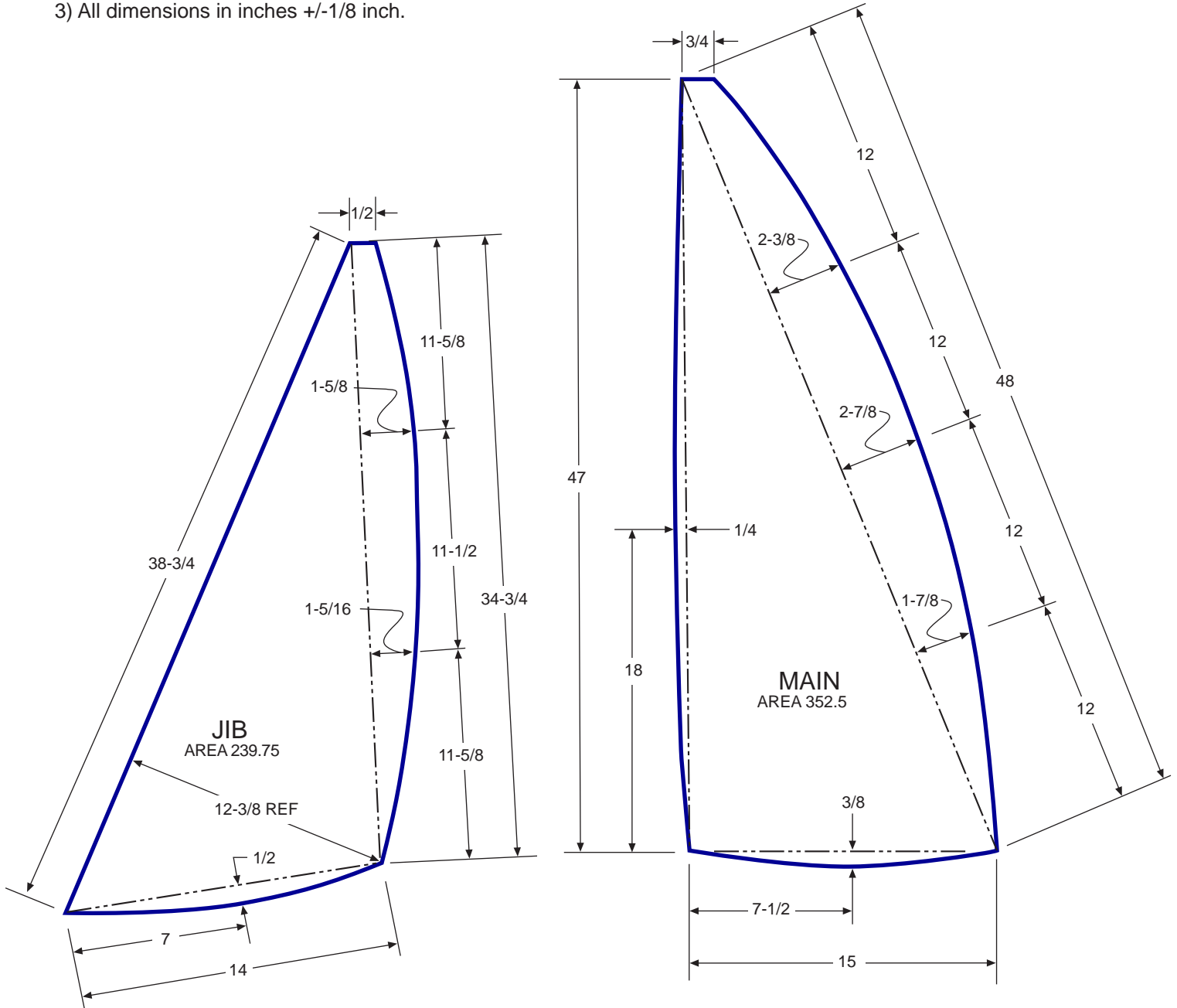


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Sail Control Drawing

Notes:

- 1) This drawing defines the shape of sails in the flat, not on the boat.
- 2) Areas are according to AMYA rule.
- 3) All dimensions in inches +/- 1/8 inch.



DO NOT SCALE DRAWING

Revision of: February 14, 2005

Allowable S1M Rigging

By Paul Fixx, AMYA Soling One Meter Class Secretary

April 2, 2007



In 2006 S1M Class members voted for changes to the Class Rules that clarified allowed rigging options.

The diamond shroud arrangement may now be replaced by a set of upper shrouds running from the top section of the mast where the jib stay attaches, through the outer ends of the spreaders and then to the deck anywhere in the range of Note 4 on the General Configuration Control Drawing. This arrangement had become common on SIMs throughout the county and is now allowed.

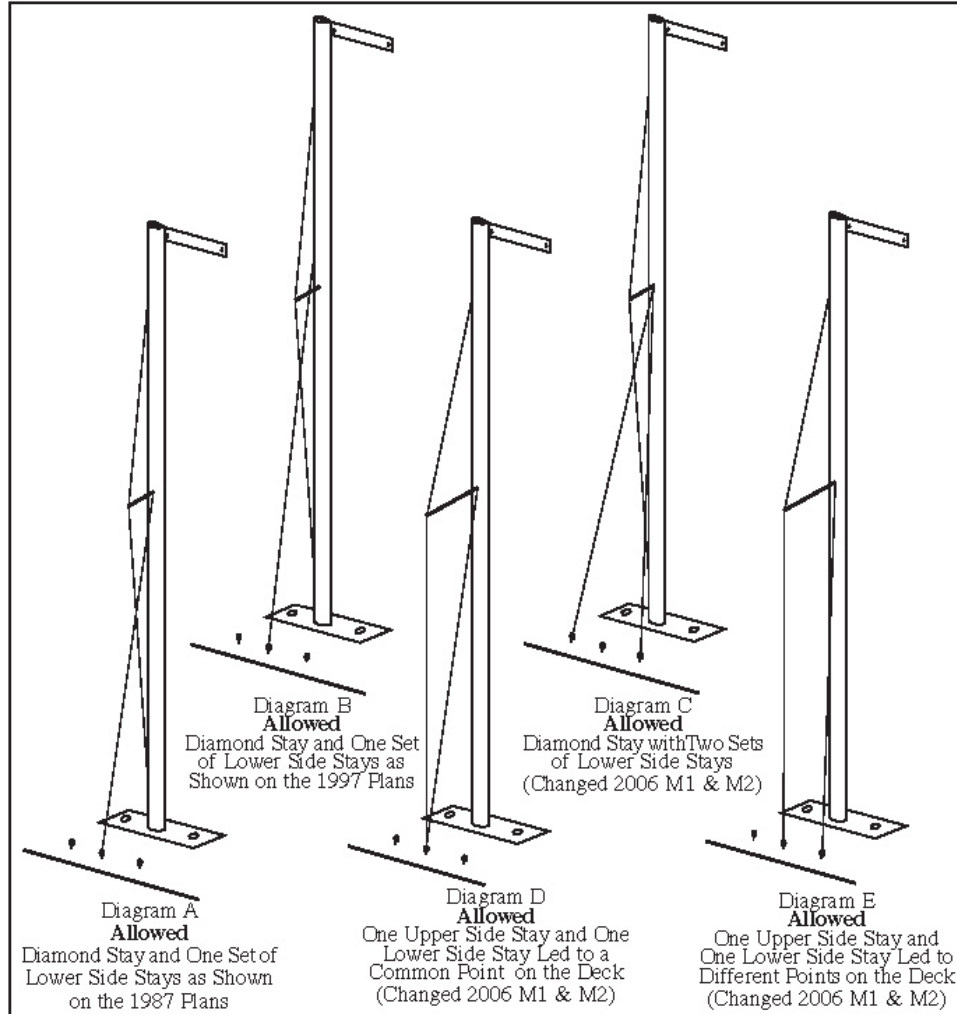
The drawings show several allowed rigging options.

Diagram A shows the mast rigged as in the original 1987 plans with the lower shrouds attached between the spreaders and the mast.

Diagram B shows standing rigging built to the 1997 plans with the lower shrouds attached to an eye on the forward edge of the mast slightly above the spreaders.

Diagram C shows a newly allowed configuration with a second set of lower shrouds that may be attached

at that spreaders, as in this diagram, or to the forward edge of the mast as in diagram B. The lower shrouds may be attached at the same or different points on deck, though always in the range of General Configuration Control Drawing Note 4. Diagram D shows the new configuration with the diamond shrouds replaced by a set of upper shrouds attached at the same point on deck as the lower shrouds. Diagram E shows the same configuration with the upper shrouds attached at a point on deck different from where the lower shrouds attach.



Another optional rigging arrangement allows for the addition of a second set of lower shrouds that will provide redundancy in the event one of them comes loose. The second set of lower shrouds may be

attached at the same point as the original set, or at different points, always within the range of General Configuration Control Drawing Note 4.

Lower shrouds may be attached either between the spreaders and the mast as in the 1987 Victor kit plans, or to the forward edge of the mast, slightly above the location of the spreaders, as in the 1997 version of the Victor kit plans.

Class Rules still allow multiple diamond rigging without specifying how that is to be achieved. Nowhere in the rules is a second set of spreaders allowed so this rule is interpreted to allow additional diamond shrouds running from the mast, through the spreaders and back to the mast. An upper shroud may not terminate on deck if multiple diamond shrouds are used. A second set of lower shrouds is allowed if multiple diamond shrouds are used.

With any of the allowed rigging arrangements turnbuckles may be used to attach shrouds to the deck or the mast. Shrouds attached to the deck must always be attached in the range of General Configuration Control Drawing Note 4.

Under no circumstance are spreaders allowed to angle fore or aft of the mast.